

## GENERAL INFORMATION

### Subject

Request	-	Special Use Permit for a new Community Center
Location	-	601 Junction Avenue
Applicant / Owner	-	Lucas County Land Bank One Government Center, Suite 580 Toledo, OH 43604
Engineer	-	Frederick Associates 4645 N. Summit Street Toledo, OH 43611

### Site Description

Zoning	-	CR / Regional Commercial
Area	-	±1.47 acres
Frontage	-	±415.84' along Nebraska Avenue ±331.25' along Junction Avenue
Existing Use	-	Vacant St. Anthony's Church & Vacant Parcels
Proposed Use	-	Community Center with Parking Lot

### Area Description

North	-	CR & RD6 / Vacant Commercial, Lodge, Single-Dwelling Residential
South	-	CR & RD6 / Nebraska Avenue, Junction Park, Urban Agriculture, Single-Dwelling Homes, Duplex Homes
East	-	CR & RD6 / Residential Facility, Single-Dwelling Homes, Commercial Structure
West	-	CR / Padua Center, Single-Dwelling Homes

### Parcel History

Z-256-75	-	Zone Change from C-3 (Commercial) and M-1 (Restricted Industrial) to R-3 (Two-Family Residence) at Nebraska Ave. between Junction Ave. & Hoag St. (P.C. deferred indefinitely 11/12/75)
T-56-18	-	Lot Split at 1416 Nebraska Avenue (Administratively approved 7/18/18)

## GENERAL INFORMATION

### Applicable Plans & Regulations

- Toledo Municipal Code (TMC), Part Eleven: Planning and Zoning Code
- Forward Toledo Comprehensive Land Use Plan
- Junction Neighborhood Master Plan

## STAFF ANALYSIS

The applicant is requesting a Special Use Permit for a Community Center at 601 Stickney Avenue. The site is approximately 1.5 acres and currently zoned Regional Commercial (CR). The applicant – the Lucas County Land Bank – is proposing to convert the existing vacant church on the site into a community center which will be owned and operated by the City of Toledo, Department of Parks and Youth Services. A Special Use Permit is required to operate a community center. To the north of the site are vacant commercial parcels, a lodge, and single-dwelling residences, to the south of the site is Nebraska Avenue, Junction Park, urban agriculture, single-dwelling homes, and duplex homes, to the east is a residential group home, single-dwelling homes, and a commercial structure, and to the west is the Padua Center and more single-dwelling homes.

### Parking, Loading and Access

The site currently shares a small parking lot with the adjacent Padua Center to the west. The applicant is proposing fifty-three (53) additional parking spaces – four (4) accessible spaces adjacent to the church and forty-nine (49) spaces in a new parking lot across Junction Avenue. The Plan Director has determined that this is more than an acceptable number of parking spaces per TMC§1107.0600 *Off-Street Parking Schedule “D”*.

All auto accessible parking spaces must have a five foot (5') loading aisle per TMC§1107.1702(A). One (1) of the proposed accessible spaces is missing this loading aisle. The addition of this aisle on a revised site plan has been included as a condition of approval.

TMC§1107.0900 *Bicycle Parking* requires a minimum of one (1) bicycle parking space per ten (10) vehicle parking spaces. Six (6) bicycle parking spaces are required for the site. The proposed bicycle rack location is acceptable as depicted on the site plan. Bicycle rack(s) must be securely fixed to the ground and meet the standards of TMC§1107.0902. This has been included as a condition of approval.

The Division of Traffic Management has objected to the proposed driveway access onto Junction Avenue and believes that effective parking lot access can be achieved solely off of Nebraska Avenue. Should access to Junction Avenue be desired, the applicant may be able to gain access via the existing alley – although they would have to improve the alley in order to do so. The removal of the proposed Junction Avenue driveway access has been included as a condition of approval. This condition goes on to permit the applicant to access the adjacent alley subject to review and approval by the Plan Director and Division of Traffic Management.



**STAFF ANALYSIS (cont'd)**Parking, Loading and Access (cont'd)

The Division of Traffic Management also objects to the proposed mid-block pedestrian crossing on Junction Avenue and asks that pedestrians be directed to the signalized crossing at the corner of Nebraska and Junction. Staff agrees that pedestrian utilization of this existing signalized crossing is preferred; however, there are concerns that the proposed detention area will naturally direct pedestrians away from this preferred crossing, leading to midblock crossings whether formalized or not. Ideally the detention area would be moved either underground or to the rear of the site; however, there would be significant cost in doing so as the site naturally drains towards the proposed detention area. Traffic Management's condition to remove the mid-block crossing has been included as a condition of approval.

Landscaping and Screening

TMC§1108.0202 *Frontage Greenbelt* requires a fifteen foot (15') wide frontage greenbelt along both Junction Avenue and Nebraska Avenue. At least one (1) tree is required within these frontage greenbelts for every thirty feet (30') of frontage. Four (4) trees are required along Junction Avenue and nine (9) trees are required along Nebraska Avenue. The site plan depicts thirty-foot (30') wide frontage greenbelts with two (2) trees along Junction Avenue and six (6) trees along Nebraska Avenue. Five (5) additional trees are required within these frontage greenbelts. The addition of these required trees on a revised landscape plan has been included as a condition of approval.

TMC§1108.0202 *Frontage Greenbelt* also requires a solid evergreen hedge planting within the frontage greenbelts when abutting a parking lot. Low, undulating (horizontal and vertical) earth berms or decorative stone or brick walls (no concrete block), or ornamental metal fencing or combinations thereof may also be approved by the Planning Director. None of these options have been proposed; however, it is required in order to screen the parking lot. The addition of a frontage greenbelt screen – whether evergreen shrubs or another method approved by the Planning Director – on a revised landscape plan has been included as a condition of approval.

Lastly, TMC§1108.0202 *Frontage Greenbelt* prohibits traditional retention and detention pond stormwater treatment facilities within the required frontage greenbelt. Other stormwater treatment facilities may occupy only up to fifty percent (50%) of the frontage greenbelt. A swale stormwater treatment facility is proposed in the majority of the frontage greenbelt. A small portion of the facility – the area labeled “micropool” on the attached parking lot grading plan – is considered a traditional retention/detention pond. TMC§1108.0202 permits the Plan Director to waive these requirements if, in consultation with the City of Toledo's Division of Engineering Services, it is determined that physical site characteristics or constraints cause placement of these facilities elsewhere impractical. The Plan Director is willing to waive this requirement in this fashion so long as a portion of the stormwater facility is planted as a low maintenance pollinator garden. The submission of low maintenance pollinator garden details to the Plan Director for review and approval has been included as a condition of approval.

**STAFF ANALYSIS (cont'd)**

Landscaping and Screening (cont'd)

A Type A landscape buffer is required along the parking lot's eastern property line per TMC§1108.0203 due to the adjacent property's Duplex Residential (RD6) zoning classification. A ten foot (10') wide landscape area with a solid fence or wall with a minimum height of six feet (6') is required in this area. Four (4) trees and fifteen (15) shrubs are required for every 100 linear feet. As such, five (5) canopy trees and nineteen (19) shrubs are required within this eastern Type A buffer area. No trees or shrubs are shown on the landscaping plan within the provided ten foot (10') wide landscape buffer. The addition of these trees and shrubs on a revised landscape plan has been included as a condition of approval.

TMC§1108.0204 *Parking Lot Landscaping* requires two (2) canopy trees and six (6) shrubs for every ten (10) parking spaces within a parking lot. Ten (10) trees and thirty (30) shrubs are required in this forty-nine-space (49) parking lot. Six (6) trees and thirty-three (33) shrubs are shown on the landscaping plan. The addition of these required interior parking lot trees has been included as a condition of approval.

Frontage greenbelt trees have been added on the community center building's side of the street as well. This is an existing site with minimal proposed modifications along the right-of-way. A new frontage greenbelt is not required; however, the proposed trees bring the site closer to conformance with the code and further the goals of the Junction Neighborhood Master Plan.

Also on the community center building's side of the street is a proposed eight-foot (8') tall wooden screening fence with shrubs surrounding several ground-mounted mechanical units.

Junction Neighborhood Master Plan

The Junction Neighborhood Master Plan identifies St. Anthony's Church as an iconic monument for both the Junction Neighborhood and Toledo as a whole. The plan emphasizes that this church should remain a vital part of the neighborhood and that it should serve as a cultural core. The plan also states that reuse of the church should establish revenue for the neighborhood for continual neighborhood development. The conversion of the church into a community center would promote the building as a vital cultural core for the Junction neighborhood. The community center use is compliant with the Junction Neighborhood Master Plan.

Forward Toledo Comprehensive Land Use Plan

The Forward Toledo Comprehensive Land Use Plan identifies this site for neighborhood mixed-use land uses. The Neighborhood Mixed-Use land use designation preserves and promotes pedestrian-oriented commercial and mixed-use areas. These are a core component of neighborhoods and provide the highest density of commercial and housing opportunities. Individual parking lots for single commercial uses are discouraged. Parking areas should be consolidated serving multiple users. The proposed community center use is consistent with the Forward Toledo Plan as the community center will fill a longstanding need in the Junction

**STAFF ANALYSIS (cont'd)**

Forward Toledo Comprehensive Land Use Plan (cont'd)

neighborhood for a flexible space that can host activities like athletic training, youth programming, community meetings, and other private events. Additionally, the proposed development supports the following goals of Forward Toledo: Promote Recreational Opportunities, Encourage Social Opportunities, Building Preservation and Strengthen Neighborhoods.

Staff recommends approval of the requested Special Use Permit because the proposed use meets the stated purpose of the zoning code and is compatible with adjacent uses in terms of scale, site design, and operating characteristics. Additionally, staff recommends approval of the proposed community center because it is supported by multiple goals of the Forward Toledo Comprehensive Land Use Plan.

**STAFF RECOMMENDATION**

The staff recommends that the Toledo City Plan Commission recommend approval of SUP25-0022, a Special Use Permit for a new community center at 601 Junction Avenue, to the Toledo City Council, for the following three (3) reasons:

1. The proposed use meets the stated purpose of the Zoning Code and is consistent with the recommendations and goals of the Forward Toledo Comprehensive Land Use Plan (TMC§1111.0706(A));
2. The proposed use complies with all applicable provisions of the zoning code (TMC§1111.0706(B)); and
3. The proposed use is compatible with adjacent uses in terms of scale, site design, and operating characteristics (TMC§1111.0706(C)).

The staff further recommends that the Toledo City Plan Commission recommend approval of SUP25-0022, a Special Use Permit for a new community center at 601 Junction Avenue, to the Toledo City Council, with the following **forty-two (42)** conditions:

The following conditions are listed by agency of origin. Applicants are encouraged to contact each of the agencies to address compliance with their conditions.

Bureau of Fire Prevention

1. It appears that this building will undergo a change of use and change in level of activity. This will require compliance with all applicable Building, Fire, Electrical, Mechanical and Plumbing code requirements. (OBC 105.1 & 101.4)
2. New and existing buildings shall be provided with approved address identification that meets Building Fire Code requirements. (OBC 501.2 & OFC 505.1)

**STAFF RECOMMENDATION (cont'd)**

Bureau of Fire Prevention (cont'd)

3. A radio signal strength test will be needed in the building, and possible radio signal enhancements based on the results of the test. (OFC 510.2 & 1103.2 – for existing buildings)
4. A fire safety inspection is required prior to occupancy. (OFC 105.3.3.2 & TMC 1309.09)
5. Fire Safety during remodeling shall comply with the applicable requirements of the Building and Fire Codes. (OBC & OFC Chapter 33)

Division of Transportation

6. All auto accessible parking spaces must have a 5' loading aisle per TMC 1107.1702 (A). (One auto accessible parking space does not have a loading aisle.)
7. Accessible parking signage must be posted directly in front of the parking space at a height of no less than 60 inches and no more than 72 inches above pavement level per TMC 1107.1704. (Accessible parking sign detail is too tall.)
8. All parking spaces, driveways and drive aisle must be clearly dimensioned on drawings per TMC 1107.1911. (Existing parking near new accessible parking is not dimensioned.)
9. Drive aisles are required to be 25' wide for two-way traffic per TMC 1107.1911.
10. Adjacent parking lot shall direct pedestrians to corner of Nebraska and Junction to promote utilizing signalized crossing. The mid-block crossing shall not be permitted for safety concerns of pedestrians.
11. Traffic Management will not permit Junction Avenue driveway access for the adjacent parking lot.
12. Any and all unused curb cuts and drive approaches must be removed on Junction Avenue and replaced with new curb and grass to match adjacent areas.

Division of Engineering Services

13. All **proposed** sidewalk, drive approaches, curb, and pedestrian curb ramps within the public right-of-way shall be constructed in accordance with the City of Toledo Construction Standards, Specifications, Toledo Municipal Code, and Americans with Disabilities Act guidelines.

**STAFF RECOMMENDATION (cont'd)**

Division of Engineering Services (cont'd)

14. All **proposed** commercial drive approaches, (along with the sidewalk through the drive) shall be constructed with 8" thick concrete per City of Toledo Construction Standards and Specifications. No curb shall be permitted in the right-of-way along either side of a drive approach. **No horizontal sub cutting will be permitted to create a curb drop/opening.** Existing drive approaches, including the curb drop, that will no longer be utilized shall be removed and restored with curb to match the adjacent curb.
15. Required permits for all approved work in the public right-of-way shall be obtained, before work begins, from Ted Rousos, 419-245-1347. The application can be obtained online at Toledo.oh.gov search: Right of Way opening permit.
16. Pavement Repair areas on Junction Ave. and in the alley need to match existing pavement.
17. The City of Toledo public water system does not appear to be impacted by this project. Refer to comments provided by the Division of Water Distribution regarding private water mains and/or service lines.
18. Designer should consider potential for sewer gas odor at proposed new downspout connections.
19. Basin footprint and size will need confirmed to provide for the required flow restriction, flood control volume, and side slope grading.
20. A full submittal for review requires submittal of the regional SWP3 submittal coversheet as provided to applicant, and its items 1.a. – 1.g. Following the stormwater review, comment and revisions, additional items needed to receive a stormwater construction permit are listed on the SWP3 submittal cover sheet, items 2.a. – 2.c., as well as the sewer fee for plan review and field inspection.
21. SWP3 activities and sequencing shall account for demolition, since demolition permitting could occur prior to construction permitting.
22. Sanitary sewer service for this development is available subject to the Rules and Regulations of the Department of Public Utilities.
23. A single sanitary sewer tap from this site shall be allowed into the public sanitary sewer system. Developer shall use existing sanitary tap, when available.
24. It is recommended for the owner to modernize the property with a sewage check valve to protect against sewage backup that could occur in the event of surcharge of the City sewer main or church downspouts.

**STAFF RECOMMENDATION (cont'd)**

Division of Environmental Services

25. Applicant shall maintain compliance with the City of Toledo's Storm Water regulations as specified in the Toledo Municipal Code. Special attention must be paid to all potential storm water impacts from the modification of the site, including but not limited to long-term operation and maintenance of existing structural and non-structural Best Management Practices.
  - a. Notification shall be made to the Division of Environmental Services (419-936-3015) no later than three days prior to commencement of construction activities.
  - b. Construction BMPs shall be in place prior to the start of construction activities.
  - c. SWP3 inspection reports shall be kept on site with the SWP3 and readily accessible during normal working hours. (this comment is specific for sites above 2,500 square ft)
26. Applicant shall maintain compliance with Ohio EPA's General Storm Water NPDES permit programs.
27. Applicant is strongly encouraged to install permanent inlet filters with oil absorbent pillow in parking lot catch basins to treat stormwater.
28. Applicant is strongly encouraged to include multiple green infra-structure measures to minimize runoff and increase infiltration, and to minimize the amount of new and/or additional impervious surface on the site.
29. Applicant is strongly encouraged to plant native, low maintenance and non-invasive trees, shrubs and perennials. Information is at <https://ohiodnr.gov/wps/portal/gov/odnr/discover-and-learn/plants-trees/invasive-plants>; a list of invasive plants and alternative species can be downloaded from [https://www.oipc.info/uploads/5/8/6/5/58652481/alternatives\\_to\\_ohio\\_invasive\\_plant\\_species.pdf](https://www.oipc.info/uploads/5/8/6/5/58652481/alternatives_to_ohio_invasive_plant_species.pdf)
30. Applicant shall maintain compliance with the City of Toledo and the State of Ohio's Air Quality Regulations applicable in the Toledo Municipal Code and the Ohio Administrative Code including, but not limited to the Asbestos and the Anti-Noise Laws.

Plan Commission

31. Bicycle spots must be at least two feet (2') by six feet (6') and rack(s) must be securely fixed to the ground per TMC§1107.0900. **Detail shall be submitted to the Plan Director for review and approval.**

**STAFF RECOMMENDATION (cont'd)**

Plan Commission (cont'd)

32. Pursuant to TMC§1108.0202 the frontage greenbelts along Junction and Nebraska Avenues adjacent to the proposed parking lot require least one (1) tree for every thirty feet (30') of frontage. **Not acceptable as depicted. A revised landscape plan shall be submitted to the Plan Director depicting at least four (4) trees within Junction Avenue greenbelt and nine (9) trees within Nebraska Avenue greenbelt.**
33. Pursuant to TMC§1108.0202 the frontage greenbelts along Junction and Nebraska Avenues adjacent to the proposed parking lot require a solid evergreen hedge planting. Low earth berms, decorative stone or brick walls, ornamental metal fencing, or combinations thereof may also be approved by the Planning Director. **Not acceptable as depicted. A revised landscape plan shall be submitted to the Plan Director depicting an acceptable screen within the Junction Avenue and Nebraska Avenue greenbelts.**
34. Portions of the proposed stormwater treatment facility shall be planted with a low maintenance pollinator garden. Low maintenance pollinator garden details shall be submitted to the Plan Director for review and approval.
35. A Type A landscape buffer is required along the parking lot's eastern property line pursuant to TMC§1108.0203. Four (4) canopy trees and fifteen (15) shrubs are required for every 100 linear feet. **Not acceptable as depicted. A revised landscape plan shall be submitted to the Plan Director depicting at least five (5) canopy trees and nineteen (19) shrubs within this type A buffer.**
36. Two (2) canopy trees and six (6) shrubs are required for every ten (10) parking spaces within the parking lot pursuant to TMC§1108.0204. **Not acceptable as depicted. A revised landscape plan shall be submitted to the Plan Director depicting at least ten (10) trees and thirty (30) shrubs within or adjacent to the parking lot.**
37. The city should consider permitting nearby commercial/institutional users to use the proposed parking lot at the corner of Junction Avenue and Nebraska Avenue.
38. Applicant shall obtain appropriate permits for any proposed signage, subject to TMC§1113 *Signs*.
39. The Special Use Permit may be reviewed for compliance with the conditions of approval, negative secondary effects, and adherence to all requirements of the Toledo Municipal Code. If outstanding issues are found and not addressed the Special Use Permit may be considered for revocation.
40. Approval of the Special Use Permit will lapse after one (1) year if the criteria listed in TMC§1111.0707 have not been met.

**STAFF RECOMMENDATION (cont'd)**

Plan Commission (cont'd)

41. Minor adjustments to the site plan that do not violate the above conditions, or the Toledo Municipal Code, may be reviewed and approved by the Director of the Toledo City Plan Commission.
42. No permits shall be issued until arrangements satisfactory to the Director of the City of Toledo Plan Commission have been made for compliance with the conditions as set forth above.

SPECIAL USE PERMIT  
TOLEDO CITY PLAN COMMISSION  
REF: SUP25-0022  
DATE: August 14, 2025  
TIME: 2:00 P.M.

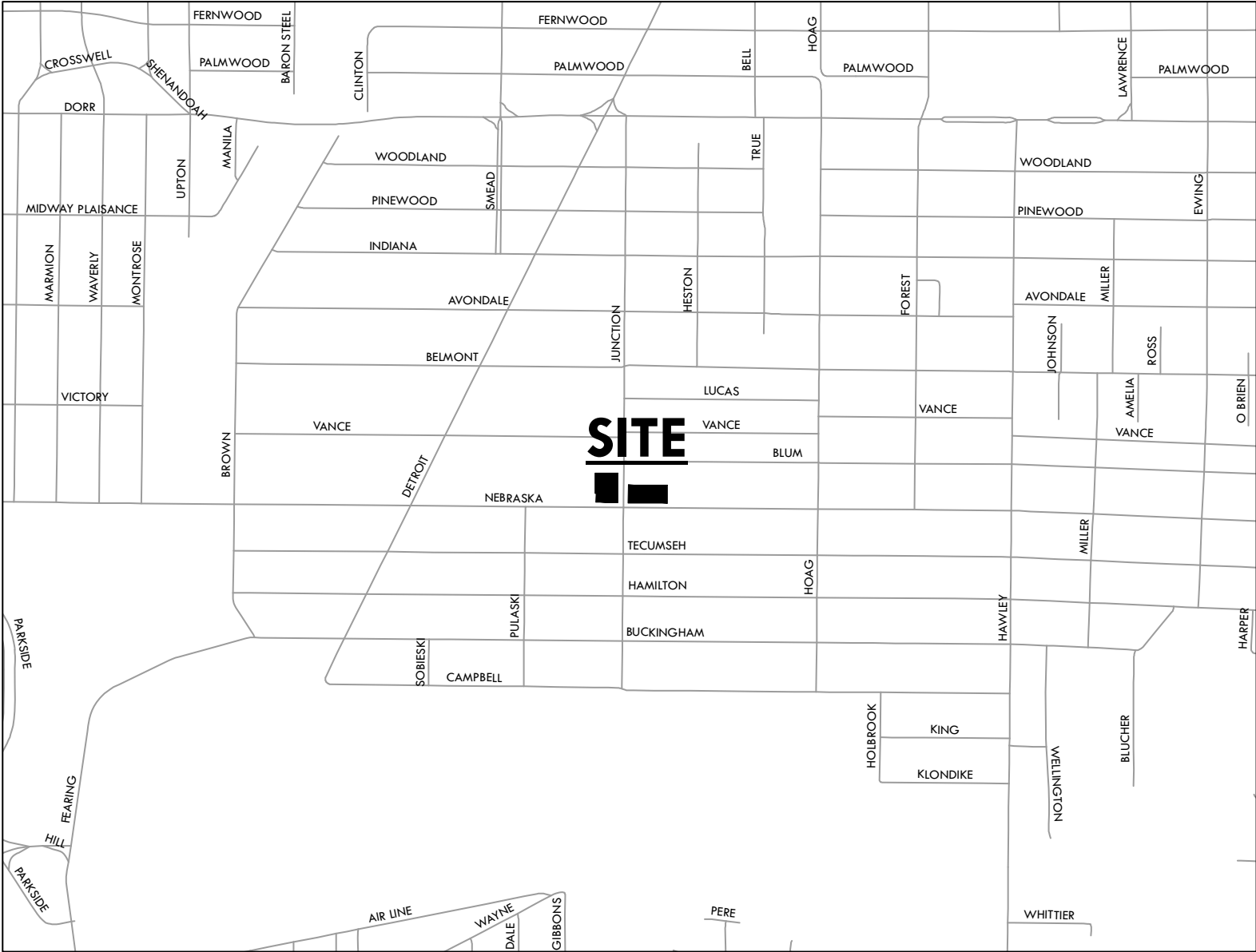
CITY COUNCIL  
ZONING AND PLANNING COMMITTEE  
DATE: September 17, 2025  
TIME: 4:00 P.M.

AS  
Seven (7) sketches follow



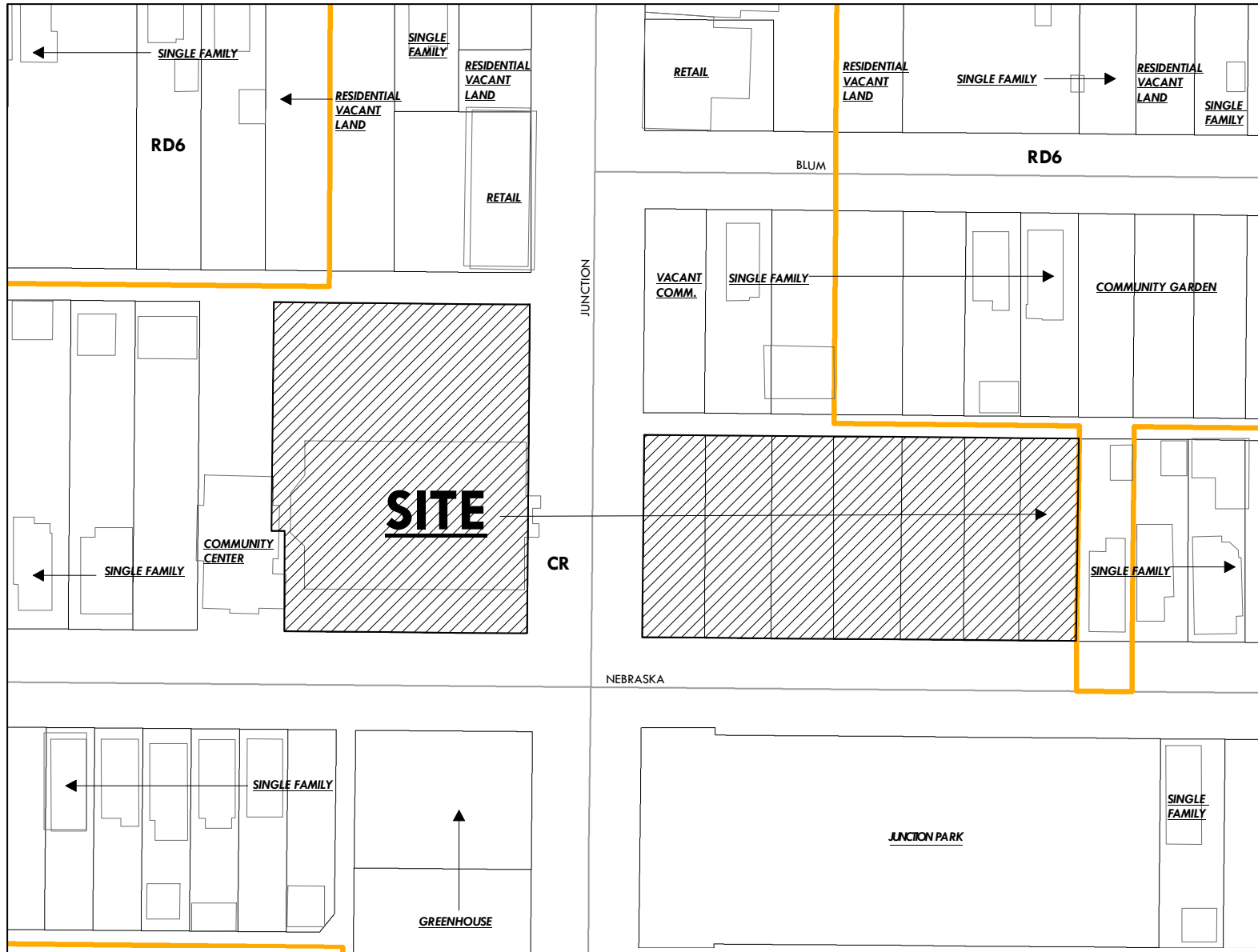
GENERAL LOCATION

SUP25-0022  
ID 19



# ZONING & LAND USE

SUP25-0022  
ID 19



# SITE PLAN A

SUP25-0022  
ID 19



## PROJECT WORK LIMITS

LIMITS OF DEVELOPMENT AREA FOR THE 2023 ST ANTHONY BUILDING RENOVATION & SITE IMPROVEMENTS, SUBJECT TO FINAL DESIGN. CONTRACTORS OPERATIONS AND COORDINATION WITH OWNER & PROJECT MANAGER.

## UNDERGROUND UTILITY LOCATION:

CONTRACTOR IS REQUIRED TO INVOKE GRS SERVICES TO SHOW AREAS OF WORK PRIOR TO ANY EXCAVATION.

## SUMMARY OF IMPERVIOUS AREAS-PROPOSED:

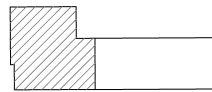
138 SF - 10 ASPHALT (JUNCTION AVE DRIVE)  
1,078 SF - 10 ASPHALT (SITE IMPROVEMENTS)  
2,884 SF - CONC EX & PROP SIDEWALK  
193 SF - 10 CONC (ALLEY REPAIR)  
12,017 SF - EX BUILDING  
16,227 SF - TOTAL IMPERVIOUS AREA PROPOSED

PER SHEET C1-10 - 7,022 SF OF EXISTING IMPERVIOUS AREA IS TO BE REMOVED.

OVERALL CONSIDERATION:  
EX CONDITION = 20,482 SF  
PROP CONDITION = 16,227 SF  
NET DECREASE IN IMPERVIOUS AREA = 20,482 SF - 16,227 SF = 4,255 SF  
REPRESENTS A 20.8% REDUCTION IN IMPERVIOUS AREA ON THE CHURCH RENOVATION SITE.

## PAVEMENT LEGEND

- 1 LIGHT-DUTY ASPHALT PAVEMENT 1,078 SF
- 2 HEAVY-DUTY ASPHALT PAVEMENT 1,354 SF
- 3 CONCRETE PAVEMENT 592 SF
- 4 HEAVY-DUTY CONCRETE 1534 SF
- 5 PARKING BLOCKS - 4 TOTAL



## KEY PLAN

N.T.S.

## LEGEND

- PROPERTY LINE
- RIGHT-OF-WAY LINE
- LOT LINE
- SETBACK LIMIT
- EASEMENT
- CENTERLINE
- EDGE OF ASPHALT
- CURB
- CONCRETE
- EXISTING BUILDING
- SANITARY SEWER
- STORM SEWER
- WATER LINE
- ELECTRIC LINE
- GAS LINE
- TELEPHONE LINE
- CABLE LINE
- FIBER OPTIC LINE
- FENCE
- CONTOUR
- POWER POLE
- EXISTING LIGHT POLE
- PROPOSED LIGHT POLE
- EXISTING SPOT ELEVATION
- PROPOSED SPOT GRADE
- MONUMENT SIGN
- ROADWAY SIGN
- ELECTRIC DROP
- SANITARY MANHOLE
- STORM MANHOLE
- CATCH BASIN
- WATER VALVE
- FIRE HYDRANT
- WATER METER
- SPRINKLER HEAD
- PLANT TREE
- DECIDUOUS TREE
- BUSH
- TRAFFIC PULL BOX
- TELEPHONE PEDESTAL
- GAS METER
- ELECTRIC METER
- PULL BOX
- MAILBOX
- IRON PIPE FOUND
- IRON PIN FOUND
- PE NAIL FOUND
- IRON PIN SET
- PE NAIL SET
- HUB SET
- DRILL HOLE SET

## GENERAL NOTES:

- 1) ALL DIMENSIONS ARE SHOWN TO EDGE OF PAVEMENT / FACE OF CURB, UNLESS OTHERWISE NOTED.
- 2) CONTRACTOR SHALL NOT SCALE ANY DIMENSIONS FROM THE PLANS. ANY QUESTIONS AND/OR CLARIFICATIONS SHALL BE SUBMITTED TO THE ENGINEER PRIOR TO BEGINNING CONSTRUCTION.
- 3) CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING POSITIVE DRAINAGE OVER THE SURFACES OF THE PROJECT TO THE STORM SEWER SYSTEM. COORDINATE ANY GRADING SURFACES WITH THE DEVELOPER AND ENGINEER PRIOR TO THE COMMENCEMENT OF THE WORK.
- 4) REFER TO THE STANDARD DETAIL SHEETS C4-10 FOR ADDITIONAL SPECIFICATIONS, PAVEMENT SECTIONS, ETC.
- 5) GRADES INDICATED ON PLAN ARE FOR TOP OF PAVEMENT OR FINISHED SOIL / CONCRETE AND THE BOTTOM GRADE IS FOR THE TOP OF CURB OR WALK.
- 6) EXISTING UTILITIES ARE TO REMAIN. ALL UTILITIES SHALL BE STRIPPED OF THE TOPSOIL PRIOR TO FILLING OPERATIONS. UTILITIES SHALL BE REMOVED IN A LEGAL MANNER OR STOPPED FOR FUTURE USE & REINSTATED OVER REMAINING PROPOSED GRASSED AREAS AT THE COMPLETION OF GRADING OPERATIONS.
- 7) ALL UTILITIES SHALL BE PLACED IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF THE CITY OF TOLEDO, LUCAS COUNTY AND GDOT.
- 8) MEANS TO RECEIVE FILL ARE TO BE CLEARED OF TREES, STUMPS, DEBRIS AND OBSTRUCTIONS PRIOR TO FILLING. DEBRIS TO BE REMOVED OFF-SITE IN A LEGAL MANNER. STUMPS AND TREES MAY NOT BE BURIED ON SITE.
- 9) ON COMPLETION OF THE GRADING OPERATIONS, FILL SURFACES AND LOT AREAS ARE TO BE ESTABLISHED PER THE DESIGN CONTROL STANDARDS AND SPECIFICATIONS OF THE CHD EPA, THE LUCAS COUNTY ENGINEER & THE CITY OF TOLEDO.
- 10) PROVIDE MIN 12" VERTICAL CLEARANCE BETWEEN ALL UTILITIES.
- 11) BUILDING SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE UTILITY LOCATION, HOLE AND LOCATION AND FINAL BUILDING DIMENSIONS WITH LATEST SURVEY AND REVISION OF ARCHITECTURAL PLANS. REFER ALL DISCREPANCIES TO THE ARCHITECT AND ENGINEER PRIOR TO CONSTRUCTION.
- 12) SEE PHOTOGRAPHIC PLAN BY OTHERS FOR FINAL LIGHT LOCATIONS AND DETAILS. ANNUAL LIGHTS SHOWN ON THIS SITE PLAN ARE FOR REFERENCE ONLY.
- 13) SITE CONSTRUCTION SEQUENCE SHALL BE COORDINATED WITH THE OWNER, ARCHITECT AND ENGINEER PRIOR TO CONSTRUCTION.
- 14) CONTRACTOR SHALL MAINTAIN TRAFFIC ALONG PUBLIC STREETS, SHARED DRIVES AND THROUGHFARES AT ALL TIMES THROUGHOUT CONSTRUCTION.
- 15) CONTRACTOR SHALL COORDINATE WITH THE DEVELOPER REGARDING THE ALIGNMENT OF THE ELECTRIC, TELEPHONE, CABLE AND GAS SERVICES TO THE PROPOSED BUILDING. ANY ALIGNMENTS INDICATED ON THIS PLAN ARE SCHEMATIC ONLY AND SUBJECT TO FINAL APPROVAL BY THE DEVELOPER AND UTILITY COMPANIES.
- 16) PARKING STRIPING SHALL BE 4" WIDE. TRAFFIC PAINT SHALL BE YELLOW AND IN CONFORMANCE WITH FEDERAL SPECIFICATION T-4-10A.
- 17) HANDCAP PARKING SPACE STRIPING SHALL BE 4" WIDE BLUE PAINT MEETING LOCAL REQUIREMENTS.
- 18) REFER TO PROPOSED LANDSCAPE PLAN BY OTHERS FOR DETAILS OF ANY AND ALL PROPOSED PLANTINGS.
- 19) SEE ARCHITECTURAL PLANS BY OTHERS FOR DETAILS OF THE PROPOSED BUILDING RENOVATIONS, DUMPSTER ENCLOSURE, FENCES, SIGNS, POST SLABS, ETC.
- 20) FOR CONCRETE WALLS DIRECTLY ADJACENT TO ASPHALT PAVEMENT AREAS THE INTEGRAL CURBWALK DETAIL SHALL BE UTILIZED FOR CONSTRUCTION. PROVIDE MINIMUM 1% AND MAXIMUM 2% CROWN SLOPE TOWARD PARKING LOT. SEE GRADING PLAN FOR DETAILED GRADING.

## PAVEMENT / CONCRETE RESTORATION:

- 1) EXISTING PAVEMENT SHALL BE REPLACED PER CITY OF TOLEDO REQUIREMENTS FOR JUNCTION NEBRASKA PAVEMENT SECTION (MATCH EXISTING) OR HEAVY DUTY PAVEMENT, WHICHEVER IS GREATER.
- 2) USE OF CONTROLLED SEVERITY FULL DEPTH CONCRETE IS NOT PERMITTED UNLESS APPROVED BY THE CITY OF TOLEDO AND UTILITY COMPANIES WITHIN THE PROPOSED WORK LIMITS.
- 3) EXISTING CONCRETE SHALL BE REPLACED PER CITY OF TOLEDO REQUIREMENTS FOR JUNCTION NEBRASKA SIDEWALK SECTION (MATCH EXISTING) OR LIGHT-DUTY (SIDEWALK) CONCRETE SECTION INCLUDING THICKENED EDGE AT STREET INTERSECTION.

## PARKING SUMMARY

- FOR THE ST ANTHONY'S IMPROVEMENTS:  
PARKING SPACE: 9111'  
PARKING ANGLE: 90°  
DRIVE ABLE: MIN 18' FOR ONE-WAY TRAFFIC
- PROPOSED PARKING:  
STANDARD SPACES = 49 SPACES  
HANDICAP SPACES = 4 SPACES (ADJACENT TO BLDG)  
TOTAL PARKING = 53 SPACES

## TRASH ENCLOSURE

- PER CLIENT, THE TRASH REMOVAL SHALL BE VIA ROLL-OFF CONTAINERS FOR THE PROPOSED BUILDING RENOVATIONS.
- PROVIDE EXISTING CONCRETE PAD WITH NEW ENCLOSURE WITH 1" VINYL PRIVACY FENCE & GATE FOR STORAGE OF ROLL-OFF CONTAINERS.

## JUNCTION AVENUE CROSSWALK

- INSTALL PER CITY OF TOLEDO - STANDARD PAVEMENT MARKING DRAWING #70.
- SOLID BARS: MIN 12" X 6"
- BAR SPACING: MIN 7'-0"

## MECH EQUIPMENT SCREENING

- CONTRACTOR SHALL INSTALL 1/2" TALL PRIVACY FENCE AS INDICATED ON THE SITE PLAN. REFER TO LANDSCAPE PLAN FOR DETAIL OF THE FENCE CONSTRUCTION.

## BICYCLE RACK DATA

- FOR THE ST ANTHONY'S IMPROVEMENTS:  
REQUIRED BICYCLE RACK CAPACITY = 49 = 5 SPACES

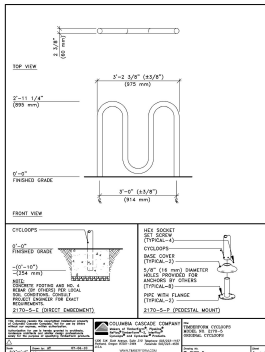
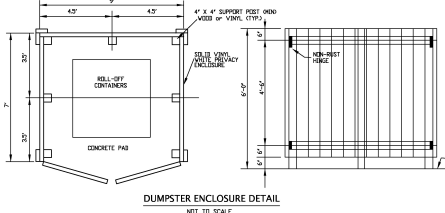
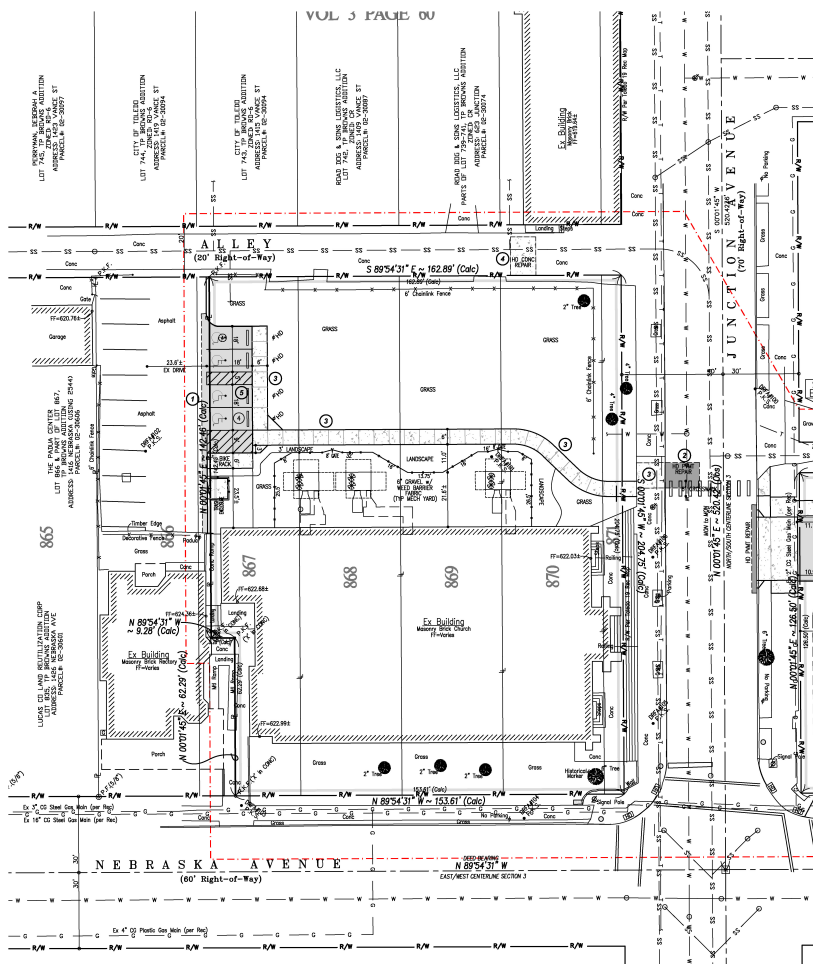
- PROPOSED BICYCLE RACKS:  
ONE (1) CONCRETE PAD INSTALLED WITH TWO (2) TUBERCON CYCLOCK MODEL NO. 2104-S RACKS  
TOTAL PROPOSED = 4 BICYCLE RACKS

## TYPICAL CONCRETE PAD

- INSTALL 8' X 16' CONCRETE - MIN 6" DEPTH

## BICYCLE RACK LEGEND

- PROPOSED EXIST CON PAD
- BICYCLE RACK
- Concrete Bicycle Pad
- 8" Non-Reinforced Concrete (3,000 PSI)
- 6" (2007) 204 Aggregate Base
- Compacted Sub-grade



THE COLLABORATIVE



DEAN R. FREDERICK  
OHIO PROFESSIONAL ENGINEER E-61807

FREDERICK ASSOCIATES  
ENGINEERS - SURVEYORS - PLANNERS

PROJECT TITLE  
LUCAS COUNTY LAND BANK

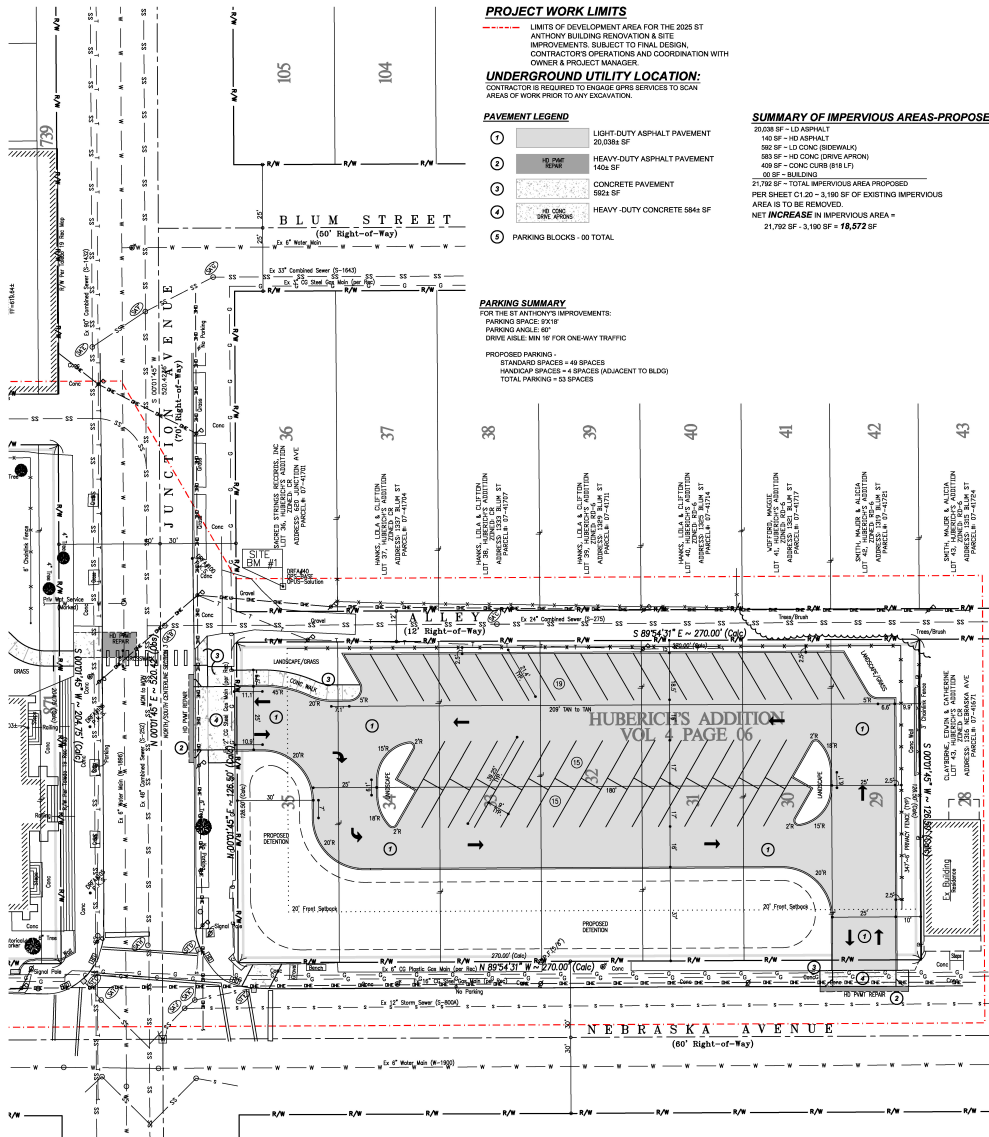
St. Anthony's Church  
Renovation  
801 Junction Avenue  
Toledo, Ohio 43607

SHEET TITLE  
SITE DIMENSIONAL PLAN

SHEET NO.  
C2.10

# SITE PLAN B

SUP25-0022  
ID 19



**PROJECT WORK LIMITS**  
LIMITS OF DEVELOPMENT AREA FOR THE 2025 ST ANTHONY BUILDING RENOVATION & SITE IMPROVEMENTS, SUBJECT TO FINAL DESIGN, CONTRACTOR'S OPERATIONS AND COORDINATION WITH OWNER & PROJECT MANAGER.

**UNDERGROUND UTILITY LOCATION:**  
CONTRACTOR IS REQUIRED TO ENGAGE SPRS SERVICES TO SCAN AREAS OF WORK PRIOR TO ANY EXCAVATION.

**PAVEMENT LEGEND**

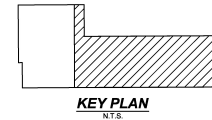
1	LIGHT DUTY ASPHALT PAVEMENT 20,000 SF
2	HEAVY DUTY ASPHALT PAVEMENT 1400 SF
3	CONCRETE PAVEMENT 5802 SF
4	HEAVY DUTY CONCRETE 6844 SF
5	PARKING BLOCKS - 50 TOTAL

**PARKING SUMMARY**  
FOR THE ST ANTHONY'S IMPROVEMENTS:  
PARKING SPACE 67X17  
PARKING ANGLE 80°  
DRIVE AISLE MIN 16' FOR ONE-WAY TRAFFIC

**PROPOSED PARKING:**  
STANDARD SPACES = 49 SPACES  
HANDICAP SPACES = 4 SPACES (ADJACENT TO BLDG)  
TOTAL PARKING = 53 SPACES

**SUMMARY OF IMPERVIOUS AREAS-PROPOSED:**

20,000 SF - LD ASPHALT
1400 SF - HD ASPHALT
5802 SF - LD CONC (SIDEWALKS)
5802 SF - HD CONC (DRIVE APRON)
400 SF - CONC CURB (814 LF)
50 SF - BUILDING
21,792 SF - TOTAL IMPERVIOUS AREA PROPOSED
PER SHEET C1.20 - 3,190 SF OF EXISTING IMPERVIOUS AREA IS TO BE REMOVED
<b>NET INCREASE IN IMPERVIOUS AREA =</b>
<b>21,792 SF - 3,190 SF = 18,602 SF</b>



**LEGEND**

PROPERTY LINE	ELECTRIC DROP
RIGHT-OF-WAY LINE	SANITARY MANHOLE
LOT LINE	CATCH BASIN
BETACK LIMIT	WATER VALVE
EASEMENT	FIRE HYDRANT
CENTERLINE	WATER METER
EDGE OF ASPHALT CURB	SPRINKLER HEAD
CONCRETE	PINE TREE
EXISTING BUILDING	DECIDUOUS TREE
SANITARY SEWER	BUSH
STORM SEWER	TRAFFIC PULL BOX
WATER LINE	TELEPHONE PEDESTAL
ELECTRIC LINE	GAS METER
GAS LINE	ELECTRIC METER
TELEPHONE LINE	PULL BOX
CABLE LINE	MAILBOX
FIBER OPTIC LINE	IRON PIPE FOUND
FENCE	IRON PIN FOUND
POWER POLE	IRON PIN SET
EXISTING LIGHT POLE	
PROPOSED LIGHT POLE	
EXISTING SPOT ELEVATION	
PROPOSED SPOT GRADE	
MONUMENT SIGN	
HIGHWAY SIGN	

**GENERAL NOTES:**

- ALL DIMENSIONS ARE SHOWN TO EDGE OF PAVEMENT / FACE OF CURB, UNLESS OTHERWISE NOTED.
- CONTRACTOR SHALL NOT SCALE ANY DIMENSIONS FROM THE PLANS. ANY QUESTIONS AND/OR CLARIFICATIONS SHALL BE SUBMITTED TO THE ENGINEER PRIOR TO BIDDING AND/OR CONSTRUCTION.
- CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING POSITIVE DRAINAGE OVER THE SURFACES OF THE PROJECT TO THE STORM SEWER SYSTEM. COORDINATE ANY GRADING SITES WITH THE DEVELOPER AND ENGINEER PRIOR TO THE COMMENCING THE WORK.
- REFER TO THE STANDARD DETAIL SHEETS FOR ADDITIONAL SPECIFICATIONS, PAVEMENT SECTIONS, ETC.
- GRASSES INDICATED ON PLAN ARE FOR TOP OF PAVEMENT OR FINISHED SOIL / CONCRETE GRADE. WHERE TWO GRADES ARE GIVEN, THE TOP GRADE IS FOR THE TOP OF CURB OR WALK, AND THE BOTTOM GRADE IS FOR THE TOP OF PAVEMENT.
- EXISTING SITES AREAS TO RECEIVE FILL SHALL BE STRIPPED OF THE TOPSOIL PRIOR TO FILLING OPERATIONS. TOPSOIL SHALL BE REMOVED FROM SITE IN A LEGAL MANNER OR STOCKPILED FOR FUTURE USE & REDISTRIBUTED OVER REMAINING PROPOSED GRASSED AREAS AT THE COMPLETION OF GRADING OPERATIONS.
- FILL SHALL BE PLACED IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF THE CITY OF TOLEDO, LOCAL COUNTY AND CDDOT.
- AREAS TO RECEIVE FILL ARE TO BE CLEARED OF TREES, STUMPS, DEBRIS AND OBSTRUCTIONS PRIOR TO FILLING. TREES TO BE REMOVED OFF SITE IN A LEGAL MANNER. STUMPS AND TREES MAY NOT BE BURIED ON SITE.
- ON COMPLETION OF THE GRADING OPERATIONS, FILL SURFACES AND LOT AREAS ARE TO BE STABILIZED PER THE EROSION CONTROL STANDARDS AND SPECIFICATIONS OF THE OHIO EPA, THE LOCAL COUNTY ENGINEER & THE CITY OF TOLEDO.
- PROVIDE MIN 14" VERTICAL CLEARANCE BETWEEN ALL UTILITIES.
- BUILDING SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE UTILITY LOCATIONS, BOLLARD LOCATIONS AND FINAL BUILDING DIMENSIONS WITH LATEST RESURVEY AND REVISION OF ARCHITECTURAL PLANS. REFER ALL DISCREPANCIES TO THE ARCHITECT AND ENGINEER PRIOR TO CONSTRUCTION.
- SEE PHOTO METRIC PLAN BY OTHERS FOR FINAL LIGHT LOCATIONS AND DETAILS. ANNUAL LIGHTS SHOWN ON THIS SITE PLAN ARE FOR REFERENCE ONLY.
- SITE CONSTRUCTION SEQUENCE SHALL BE COORDINATED WITH THE OWNER, ARCHITECT AND ENGINEER PRIOR TO CONSTRUCTION.
- CONTRACTOR SHALL MAINTAIN TRAFFIC ALONG PUBLIC STREETS, SHARED DRIVES AND THOROUGHFARES AT ALL TIMES THROUGHOUT CONSTRUCTION.
- CONTRACTOR SHALL COORDINATE WITH THE DEVELOPER REGARDING THE ALIGNMENT OF THE ELECTRIC, TELEPHONE, CABLE AND GAS SERVICES TO THE PROPOSED BUILDING. ANY ALIGNMENT INDICATED ON THIS PLAN ARE SCHEMATIC ONLY AND SUBJECT TO FINAL APPROVAL BY THE DEVELOPER AND UTILITY COMPANIES.
- PARKING STRIPING SHALL BE 4" WIDE. TRAFFIC PAINT SHALL BE YELLOW AND IN CONFORMANCE WITH FEDERAL SPECIFICATION TT-G-15A.
- HANDICAP PARKING SPACE STRIPING SHALL BE 4" WIDE BLUE PAINT MEETING LOCAL REQUIREMENTS.
- REFER TO PROPOSED LANDSCAPE PLAN BY OTHERS FOR DETAILS OF ANY AND ALL PROPOSED PLANTINGS.
- SEE ARCHITECTURAL PLANS BY OTHERS FOR DETAILS OF THE PROPOSED BUILDING, DUMPSTER ENCLOSURE, PATIO WALLS, FENCES, BENCHES, FROST SLABS, ETC.
- FOR CONCRETE WALLS DIRECTLY ADJACENT TO ADJACENT PAVEMENT AREAS THE INTEGRAL CURB/WALK DETAIL SHALL BE UTILIZED FOR CONSTRUCTION. PROVIDE MINIMUM 1% AND MAXIMUM 1% CROSS SLOPE TOWARD PARKING LOT. SEE GRADING PLAN FOR DETAILED GRADING.

**PAVEMENT / CONCRETE RESTORATION:**

- EXISTING PAVEMENT SHALL BE REPLACED PER CITY OF TOLEDO REQUIREMENTS FOR JUNCTION / NEBRASKA PAVEMENT SECTION MATCH EXISTING TO HEAVY DUTY PAVEMENT, WHICHEVER IS GREATER.
- USE OF CONTROLLED DENSITY FILL (CDF) / CONCRETE IS NOT PERMITTED UNLESS APPROVED BY THE CITY OF TOLEDO AND UTILITY COMPANIES WITHIN THE PROPOSED WORK LIMIT.
- EXISTING CONCRETE SHALL BE REPLACED PER CITY OF TOLEDO REQUIREMENTS FOR JUNCTION / NEBRASKA SIDEWALK SECTION MATCH EXISTING TO LIGHT DUTY (SIDEWALK) CONCRETE SECTION INCLUDING THICKENED EDGE AT STREET INTERFACES.

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DEAN R. FREDERICK  
OHIO PROFESSIONAL ENGINEER E-61907



PROJECT TITLE  
LUCAS COUNTY  
LAND BANK

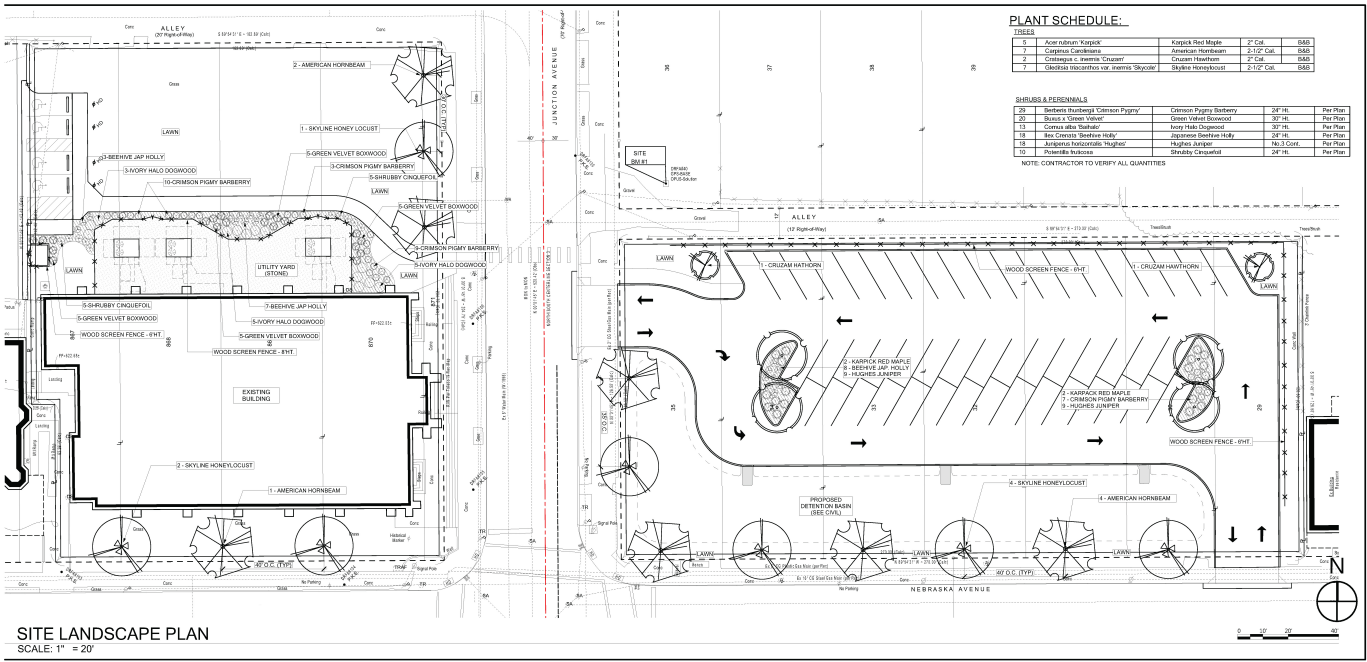
St. Anthony's  
Church  
Renovation  
801 Junction Avenue  
Toledo, Ohio 43607

SHEET TITLE  
SITE DIMENSIONAL  
PLAN

SHEET NO.  
C2.20

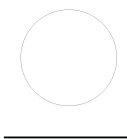
LANDSCAPE PLAN

SUP25-0022  
ID 19



- LANDSCAPE NOTES:**
- CONTRACTOR IS RESPONSIBLE FOR PLANT QUANTITIES. VERIFY ALL COUNTS WITH PLANS.
  - DIMENSIONS SHOWN FOR LARGE TREES. FORM PLANTING BEDS AND AREAS OF SEEDING ARE SUBJECT TO FIELD CONDITIONS. STAKE LINES FOR ARCHITECTS TO REVIEW BEFORE PLANTING.
  - PLANT SPACING EXPLICITLY SHOWN ON DRAWINGS SHALL TAKE PRECEDENCE OVER TYPICAL ON CENTER SPACING NOTED IN PLANT SCHEDULE.
  - ALL LANDSCAPE BEDS SHALL BE MULCHED, PREPARED AND IN ACCORDANCE WITH SPECIFICATIONS. SHOVEL OUT ALL FREE EDGES AS PER LANDSCAPE DETAILS AND SPECIFICATIONS UNLESS NOTED OTHERWISE ON DRAWINGS.
  - ALL TREES TO BE SET BY CONTRACTOR TO SUBMIT PROPOSED OPTION FOR TREE STAKING FOR APPROVAL.
  - PROVIDE BLOW-RELEASE WATERING BIDS FOR TREES AS REQUIRED BY AND IN ACCORDANCE WITH SPECIFICATIONS.
  - AREAS WITHIN THE CURBLINE OF EXISTING TREES TO REMAIN AS PART OF FINAL WORK SHALL NOT BE DISTURBED BY DRAINAGE/CONSTRUCTION ACTIVITY UNLESS NOTED OTHERWISE.

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PROJECT TITLE  
Lucas County  
Land Bank  
St. Anthony's  
Church  
Renovation

801 Junction Ave  
Toledo, OH 43607

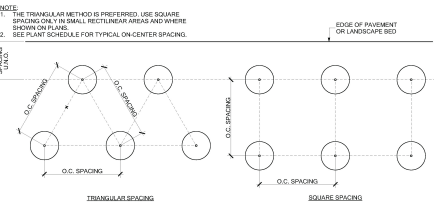
9/23/2025 FOR SITE PLAN APPROVAL

TC JOB NO. 107129  
OWNER JOB NO. 4266600

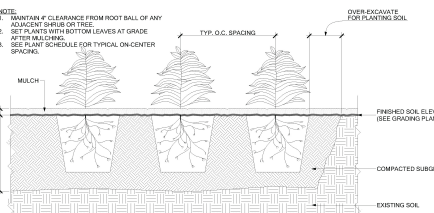
SHEET TITLE  
SITE  
LANDSCAPE  
PLAN

SHEET NO.  
L1.00

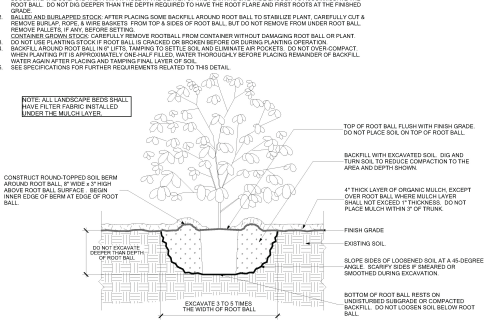
SITE LANDSCAPE PLAN  
SCALE: 1" = 20'



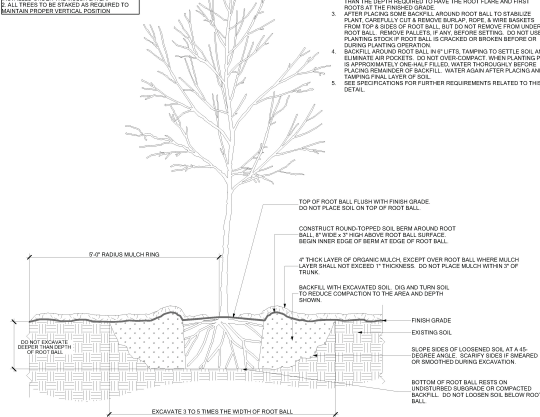
PERENNIAL/G.C. PLANTING DETAIL  
SCALE: 3/4" = 1'-0"



SHRUB PLANTING DETAIL  
SCALE: 3/4" = 1'-0"



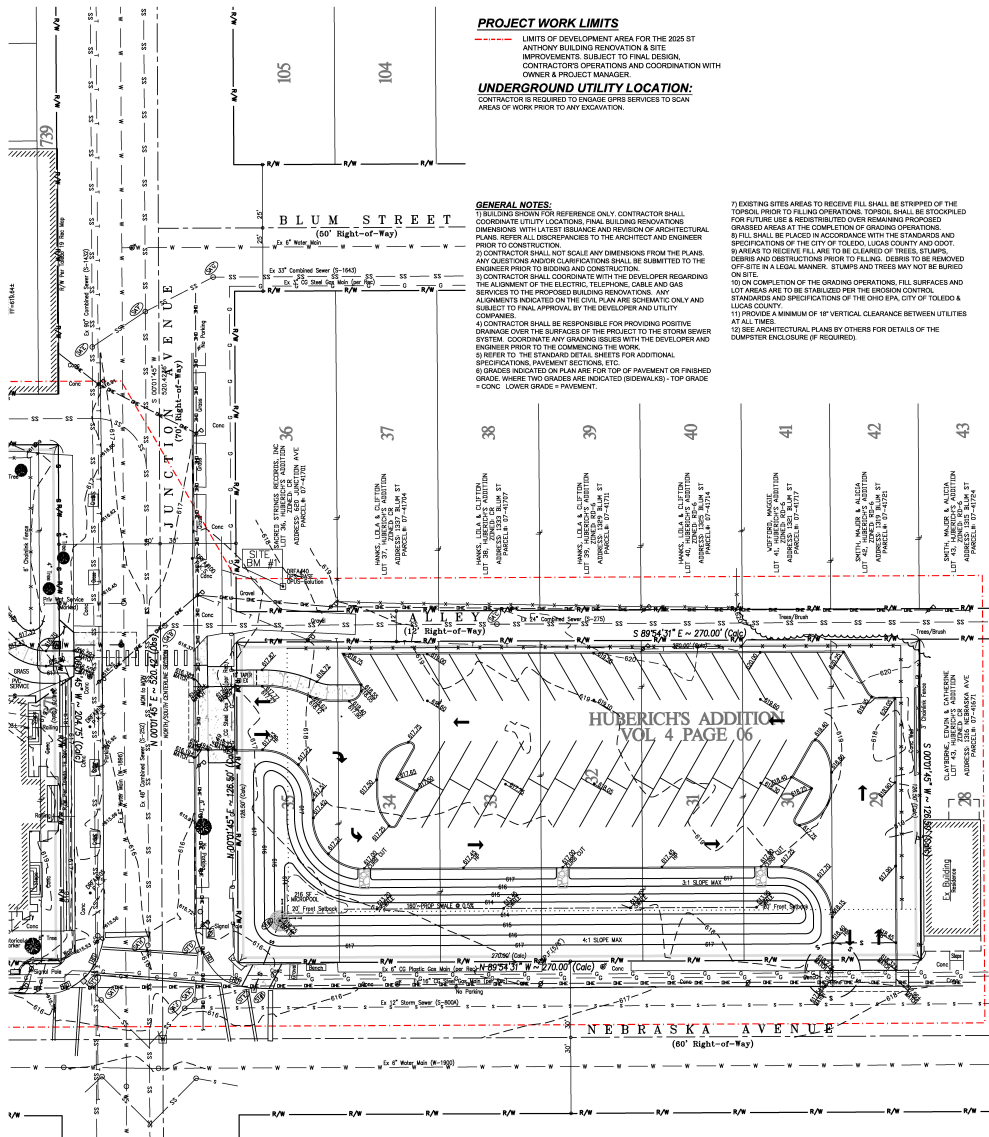
TREE PLANTING DETAIL  
SCALE: 3/4" = 1'-0"





# PARKING LOT GRADING PLAN

SUP25-0022  
ID 19



## PROJECT WORK LIMITS

LIMITS OF DEVELOPMENT AREA FOR THE 2025 ST ANTHONY BUILDING RENOVATION & SITE IMPROVEMENTS, SUBJECT TO FINAL DESIGN, CONTRACTOR'S OPERATIONS AND COORDINATION WITH OWNER & PROJECT MANAGER.

## UNDERGROUND UTILITY LOCATION:

CONTRACTOR IS REQUIRED TO ENGAGE GPRS SERVICES TO SCAN AREAS OF WORK PRIOR TO ANY EXCAVATION.

## GENERAL NOTES:

- 1) BUILDING SHOWN FOR REFERENCE ONLY. CONTRACTOR SHALL COORDINATE UTILITY LOCATIONS, FINAL BUILDING RENOVATIONS DIMENSIONS WITH LATEST EMBASANCE AND REVISION OF ARCHITECTURAL PLANS. REFER ALL DISCREPANCIES TO THE ARCHITECT AND ENGINEER PRIOR TO CONSTRUCTION.
- 2) CONTRACTOR SHALL NOT SCALE ANY DIMENSIONS FROM THE PLANS. ANY QUESTIONS AND/OR CLARIFICATIONS SHALL BE SUBMITTED TO THE ENGINEER PRIOR TO BEGINNING AND CONSTRUCTION.
- 3) CONTRACTOR SHALL COORDINATE WITH THE DEVELOPER REGARDING THE ALIGNMENT OF THE ELECTRIC, TO SPACING CABLE AND GAS SERVICES TO THE PROPOSED BUILDING RENOVATIONS. ANY ALIGNMENTS INDICATED ON THE CIVIL PLAN ARE SCHEMATIC ONLY AND SUBJECT TO FINAL APPROVAL BY THE DEVELOPER AND UTILITY COMPANIES.
- 4) CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING POSITIVE DRAINAGE OVER THE SURFACES OF THE PROJECT TO THE STORM SEWER SYSTEM. COORDINATE ANY GRADING ISSUES WITH THE DEVELOPER AND ENGINEER PRIOR TO THE COMMENCING THE WORK.
- 5) REFER TO THE STANDARD DETAIL SHEETS FOR ADDITIONAL SPECIFICATIONS, PAVEMENT SECTIONS, ETC.
- 6) GRADES INDICATED ON PLAN ARE FOR TOP OF PAVEMENT OR FINISHED GRADE, WHERE TWO GRADES ARE INDICATED (IDEALLY) - TOP GRADE = CONC. LOWER GRADE = PAVEMENT.

- 7) EXISTING SITES AREAS TO RECEIVE FILL SHALL BE STRIPPED OF THE TOPSOIL PRIOR TO FILLING OPERATIONS. TOPSOIL SHALL BE STOCKPILED FOR FUTURE USE & REDISTRIBUTED OVER REMAINING PROPOSED GRASSY AREAS AT THE COMPLETION OF GRADING OPERATIONS.
- 8) FILL SHALL BE PLACED IN ACCORDANCE WITH THE STANDARDS AND SPECIFICATIONS OF THE CITY OF TOLEDO, LUCAS COUNTY AND DODT.
- 9) AREAS TO RECEIVE FILL ARE TO BE CLEARED OF TREES, STUMPS, DEBRIS AND OBSTRUCTIONS PRIOR TO FILLING. DEBRIS TO BE REMOVED OFF-SITE IN A LEGAL MANNER. STUMPS AND TREES MAY NOT BE BURIED ON SITE.
- 10) ON COMPLETION OF THE GRADING OPERATIONS, FILL SURFACES AND LOT AREAS ARE TO BE STABILIZED PER THE EROSION CONTROL STANDARDS AND SPECIFICATIONS OF THE CHIO EPA, CITY OF TOLEDO & LUCAS COUNTY.
- 11) PROVIDE A MINIMUM OF 18" VERTICAL CLEARANCE BETWEEN UTILITIES AT ALL TIMES.
- 12) SEE ARCHITECTURAL PLANS BY OTHERS FOR DETAILS OF THE DUMPSTER ENCLOSURE (IF REQUIRED).

## STORM SEWER NOTES:

- 1) ALL STORM SEWERS TO BE INSTALLED PURSUANT TO DODT, CITY OF TOLEDO & LUCAS COUNTY STANDARDS & SPECIFICATIONS, CURRENT EDITIONS.
- 2) ALL PROPOSED STORM SEWERS SHALL REMAIN PRIVATE AFTER CONSTRUCTION.
- 3) ALL PROPOSED CATCH BASINS SHALL BE EQUIPPED WITH MIN FOUR (4) - 1" PERFORATED UNDERDRAIN PER THE DETAIL ON SHEET C-13, EACH UNDERDRAIN SHALL BE MINIMUM 10' LONG.
- 4) ALL PROPOSED CATCH BASINS SHALL BE EQUIPPED WITH MIN 2" SLOPE. ALL PROPOSED STORM SEWER LINES SHALL MAINTAIN A MINIMUM 18" VERTICAL CLEARANCE TO ALL OTHER UTILITIES.

## STORM DATA:

- SIZE  
EX ST  
3" - 15' SLOPE MAX  
4" - 15' SLOPE MAX  
6" - 15' SLOPE MAX  
8" - 15' SLOPE MAX  
10" - 15' SLOPE MAX  
12" - 15' SLOPE MAX  
15" - 15' SLOPE MAX  
18" - 15' SLOPE MAX  
21" - 15' SLOPE MAX  
24" - 15' SLOPE MAX  
30" - 15' SLOPE MAX  
36" - 15' SLOPE MAX  
42" - 15' SLOPE MAX  
48" - 15' SLOPE MAX  
54" - 15' SLOPE MAX  
60" - 15' SLOPE MAX  
72" - 15' SLOPE MAX  
84" - 15' SLOPE MAX  
96" - 15' SLOPE MAX  
108" - 15' SLOPE MAX  
120" - 15' SLOPE MAX  
144" - 15' SLOPE MAX  
168" - 15' SLOPE MAX  
192" - 15' SLOPE MAX  
216" - 15' SLOPE MAX  
240" - 15' SLOPE MAX  
270" - 15' SLOPE MAX  
300" - 15' SLOPE MAX  
324" - 15' SLOPE MAX  
348" - 15' SLOPE MAX  
360" - 15' SLOPE MAX  
378" - 15' SLOPE MAX  
396" - 15' SLOPE MAX  
414" - 15' SLOPE MAX  
432" - 15' SLOPE MAX  
450" - 15' SLOPE MAX  
468" - 15' SLOPE MAX  
486" - 15' SLOPE MAX  
504" - 15' SLOPE MAX  
522" - 15' SLOPE MAX  
540" - 15' SLOPE MAX  
558" - 15' SLOPE MAX  
576" - 15' SLOPE MAX  
594" - 15' SLOPE MAX  
612" - 15' SLOPE MAX  
630" - 15' SLOPE MAX  
648" - 15' SLOPE MAX  
666" - 15' SLOPE MAX  
684" - 15' SLOPE MAX  
702" - 15' SLOPE MAX  
720" - 15' SLOPE MAX  
738" - 15' SLOPE MAX  
756" - 15' SLOPE MAX  
774" - 15' SLOPE MAX  
792" - 15' SLOPE MAX  
810" - 15' SLOPE MAX  
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846" - 15' SLOPE MAX  
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882" - 15' SLOPE MAX  
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# NORTH ELEVATION

SUP25-0022  
ID 19



St Anthony's Parish / North Ele  
Conceptual Renderings 2025